

Application Number	15/01575/AS
Location	Site of former Klondyke Works, Newtown Road, Ashford, Kent
Grid Reference	01548/41597
Parish	None
Ward	Aylesford Green (Ashford)
Application Description	Model railway visitors centre for the display and heritage of model railway models including new highways junction and associated parking
Applicant	Mr Cliff Parsons
Agent	RDA Consulting Architects Ltd
Site Area	1.01 ha

(a) 37/1R South Willesborough And Newtown Community Group - R 1 petition (93 signatories)	(b) -	(c) HE - + KH&T - X KCC (Ecology) – R KCC (Heritage) - X SWS - X PO – X EA - Stagecoach - NR – KWT – Visit Kent – The Historical Model Railway Society – KICC -
--	-------	--

Introduction

1. The application is reported to the planning committee because it is considered to be a sensitive one with potentially wide ranging impacts and is also a major one due to the scale of the proposed floorspace.

Site and Surroundings

2. The site comprises an irregularly shaped area of mainly flat land to the south east of the town centre and railway station which was formerly in use as a railway works. The site is now cleared of buildings; the concrete foundations having been broken up and removed from the site within the last 18 months. Vegetation is confined to site boundaries, in particular the southern and eastern boundaries where the land banks up and there are tree screens. An existing vehicular access is positioned halfway along the eastern boundary of the site with access off Newtown Road.
3. The site is bordered to the west by the Ashford – Hastings railway line with which it forms a straight line boundary. Beyond the railway line is the Designer Outlet, its tented roof visible from the site. Newtown Road forms the eastern boundary: it is for most of its length at a lower level to the application site. This is most marked at the northern end of the site where Newtown Road passes beneath the Ashford-Hastings railway line. An embankment of part brick, part concrete construction forms this boundary with the application site. Beyond Newtown Road to the north of the site is an extensive area of railway land, partly cleared but including some Listed C19th railway sheds. The railway land to the north east of the site is currently being developed for housing by Kier Developments. To the south and east of the site is Wainwright Place, a residential street.
4. There are a number of Grade II listed buildings in the vicinity of the site: the closest being the former school to the south (now apartments) in Wainwright Place. A listed clock tower is situated on the other side of Newtown Road to the site access and two listed railway sheds are situated nearby on the Newtown works site.

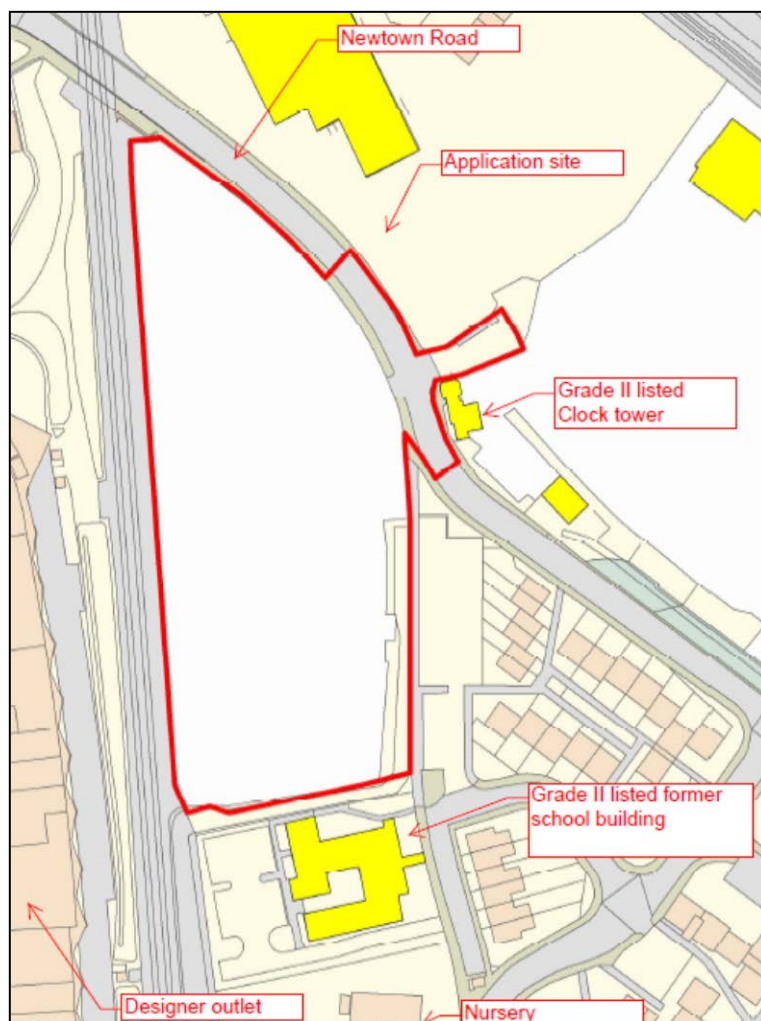


Figure 1: Site location plan

Proposal

5. The application is for full planning permission.
6. Application is made for a mostly two storey building of 3000 sq.m. to house a display area, lecture and demonstration theatre, specialist model shop, first floor cafeteria and viewing platform. The viewing platform, which forms the only three-storey element of the proposal, would provide visitors with a view of Eurostar.
7. The building would be located in the north-west corner of the site with the rest of the site used for parking; circulation and landscaping. There would also be a picnic area and miniature rideable railway which would run around the perimeter of the site.
8. The main pedestrian route into the site would extend from Newtown Road, close to the railway bridge, along the western boundary of the site to the main building entrance. Visitors would pass beneath a replica Iron Bridge structure

as they enter the site (the miniature train crosses this bridge) and past various exhibits as they approach the building along a landscaped path. The applicant has explained that this route tells a story, starting with the start of the industrial revolution and demonstrating through external exhibits and displays the role of the railways.

9. Vehicular access into the site would be achieved via a remodelled signalised junction at the existing site access on Newtown Road. A 68 space car park is proposed which would be arranged in a lozenge shape with one way traffic circulation. This would include 6 disabled bays. In addition, parking would be provided for 5 motorcycles, 3 mini buses, 2 coaches and 30 cycles. The parking area would be broken up with some landscaping and pedestrian routes running across it.

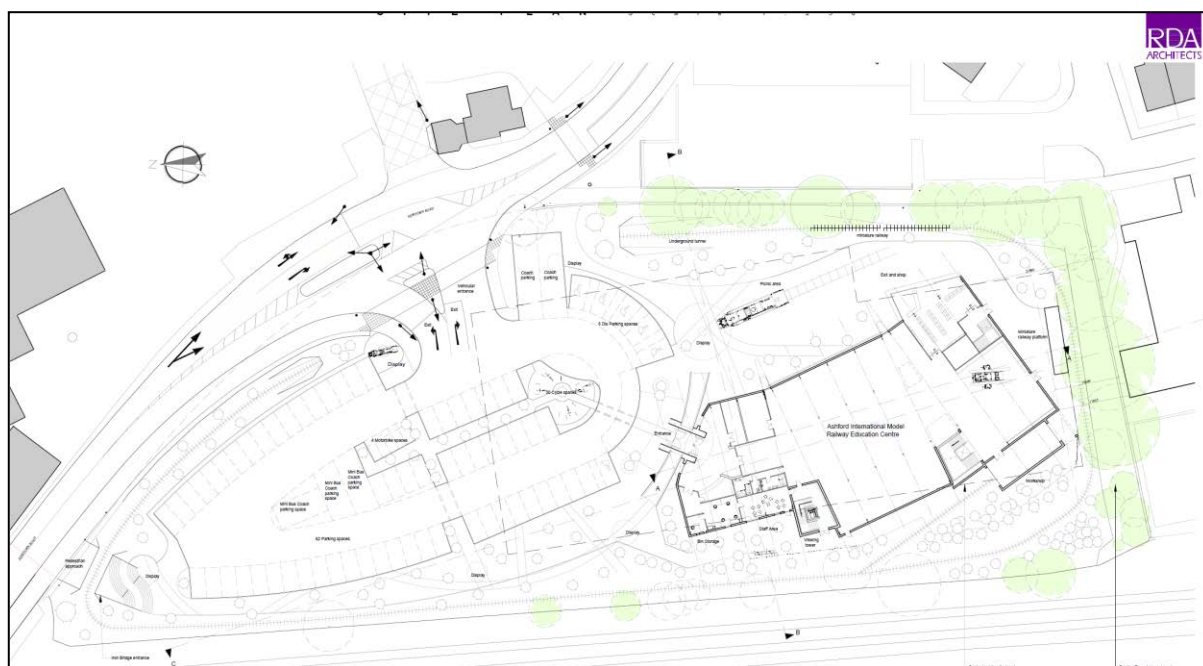


Figure 2: Site Layout

10. Visitors would enter the building through a brick tunnel which protrudes in front of the main face of the building and includes a pedestrian footbridge over the protruding element, intended to provide a feature entrance and aid pedestrian circulation around the entrance area. Within the tunnel, a CGI display continues the story of the railways whilst leading the visitor into the reception area of the building. The two storey museum space would house various model railway exhibits both temporary and permanent including a permanent display highlighting the importance of Ashford's railway heritage. A final exit display would show how railways could evolve in the future.
11. A café is provided at first floor level to the front of the building with views across the site towards the railway lines. From the café, visitors could climb up to the viewing platform. Visitors would exit the building towards the rear via

the museum shop where the landscaped picnic area and platforms to the miniature railway would be situated.

12. The building is made up of three parts which the applicant has explained are suggestive of the three main materials required for the railways: coal; metal and wood. These three blocks would curl around the corner of the site 'reminiscent of railway carriages'. They would be comprised of three steel framed boxes each with a different style of cladding: black cladding materials to the entrance building; perforated metal cladding panels to the central building and open boarded timber cladding to the rear block. The rear of the central block would be clad of aluminium cladding with mesh. Due to the fragility of the models to UV light windows would mostly be confined to the entrance, café, office and kitchen areas.
13. The three different building elements would have slightly different heights/roof profiles with the front and middle blocks having mono-pitch roofs that slope down towards the western boundary of the site and the rear block having a mono-pitch roof that slopes upwards. The maximum height of the main part of the building would be 10.5m with the top of the viewing platform extending the building to 14.75m. The building would have a length of 60m along its central axis.



Figure 3: Ground floor plan

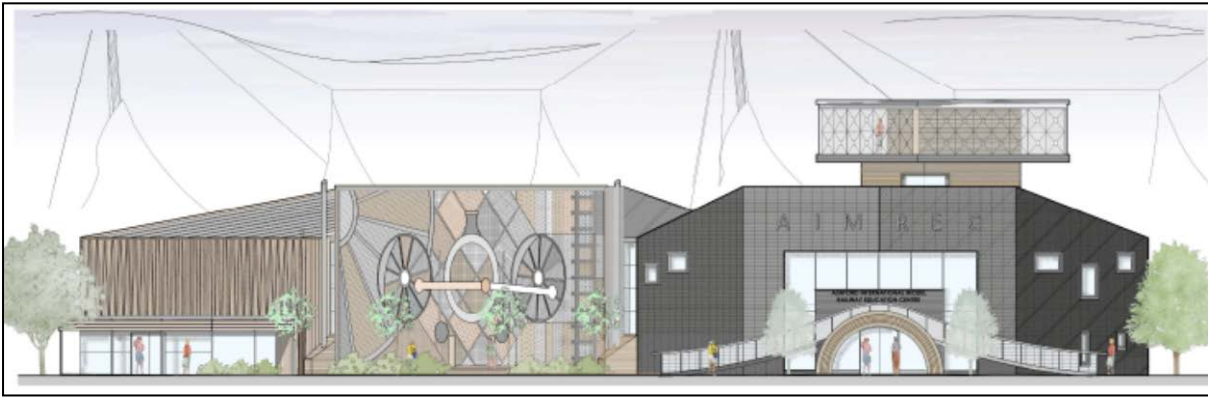


Figure 4: North East elevation - 'front'



Figure 5: South elevation



Figure 6: West elevation



Figure 7: North elevation

14. In support of the application the following have been submitted:

Archaeological Desk Based Assessment: this found the site to be located in an area of generally low archaeological potential with few archaeological and historic features that are unrelated to the railway recorded within 500 m of the site. It found that there is a high potential for post medieval remains associated with the railway to be located on the site.

Site Investigation Report: this found that the groundwater is shallow beneath the study site and advised that piled foundations are provided.

Preliminary Gas Risk Assessment: this concludes that no gas protection methods need to be incorporated within the proposed build.

Ecological Appraisal: this recommends that a Reptile Survey is carried out in suitable weather conditions between April – September. It identifies an active blue tit nest; trees with roosting bat potential and commuting /foraging potential for bats on the site. It identifies ecological avoidance, mitigation and enhancement measures.

Drainage Strategy: this advises that a combination of SUDS is used to increase the time of concentration of the water before it enters the Southern Water Sewer in Newtown Road and retaining it prior to it being discharged to the existing system. It recommends a mixture of porous paving combined with below ground and above ground storage systems. It concludes that this combination of SUDS and retention will reduce surface run-off in line with the NPPF (2012).

Transport Assessment: this concludes that the proposed development would generate traffic largely outside of the commuter peak period and hence, not cause any adverse impact on the local road network especially at commuter peak periods.

Framework Travel Plan: this aims to encourage staff and visitors to choose alternatives to car travel for journeys when they visit the centre. It is intended to appoint a Travel Plan Co-ordinator prior to occupation who will be responsible for monitoring the plan and acting as a primary contact with KCC and ABC. AIMREC's website will provide sustainable modes of travel to the centre for visitors.

BREEAM Assessment: this demonstrates that an 'Excellent' score could be achieved. It proposes that 2 credits are achieved through the introduction of LZC technologies equating to a 20% reduction in the building's CO2 emissions. No details are provided.

Planning History

15. In 2010, the Klondyke railway shed was demolished on health and safety grounds.
16. On 16th September 2015, a screening opinion was put in for the application currently under consideration. It was decided that an Environmental Impact Assessment is not required.

Consultations

Ward Members: The Ward member is not a member of the Planning Committee. No formal comments have been received.

Historic England: Recommends that the application should be determined in accordance with national and local policy guidance and on the basis of specialist conservation advice.

KCC Highways and Transportation: Required amendments to be made to the original plans /Transport Assessment in order to overcome objections. These included signalisation of the Newtown Works access as part of the interim junction design and remodelling in LINSIG. These amendments have been made and the highway authority raises no objections subject to conditions and the applicant entering into a S106 agreement as regards a contribution towards a Controlled Parking Zone in Newtown and Travel Plan.

KCC Heritage: welcomes this proposal which encourages awareness, education and enjoyment of the railway heritage in Ashford. It advises that the Deskbased Assessment by Trust for Thanet Archaeology is used to guide the design and interpretation measures for this scheme and emphasises the need for interpretation to be based on relevant archaeological data as well as documentary sources. It identifies the potential for structural remains associated with the C19th and C20th railway works to survive below the ground surface citing how archaeological investigations on the adjoining Newtown Railway works located some complete timetables and below ground structures. It recommends a number of conditions.

KCC Ecological Advice Service: has requested that a reptile survey be carried out prior to determination of the application. It also comments that as the trees have been identified as having suitable features for roosting bats, any felling will need to be carried out using a precautionary mitigation approach. (**SSDM comment:** no trees are to be felled as part of this proposal)

Southern Water: No objection subject to a condition requiring details of the proposed means of foul and surface water sewerage disposal and consideration of the following comments:

- The rainwater harvesting system, whilst acceptable in principle, must be installed and maintained to a high standard to ensure the protection of the public water supply and avoid cross contamination.
- A wastewater grease trap should be provided on the kitchen waste pipe or drain installed and maintained by the owner or operator of the premises.
- All hardstanding that may be subject to oil/petrol spillages should be drained by means of oil trap gullies or petrol/oil interceptors.
- Groundwater or land drainage will not be allowed to public sewerage network.

Environment Agency: No representations received.

Stagecoach: No representations received.

Network Rail: No representations received.

Kent Wildlife Trust: No representations received.

Visit Kent: No representations received

The Historical Model Railway Society: No representations received

Kent Invicta Chamber of Commerce: No representations received.

Project Office: No objection subject to a condition re: SUDS and consideration of the following:

- Whilst the conceptual plan and strategy is feasible, as a general comment the positioning/location of the geocellular storage unit within the car park may need further consideration at detailed design stage as the top and invert levels of the unit do not appear to be identified on the plan provided. There may be a potential clash in levels between the storage within the sub-base of the porous paving and the workable levels of the geocellular unit. However, the concept presented is deemed acceptable and it is considered more than likely that sufficient space would be available within the proposed car park to remove any interactions between porous paving and the tanked storage unit.

Neighbours: The director of Cherry Tree Nursery School (which occupies premises in Wainwright Place to the south of the application site) has raised concerns about the height of the viewing platform and the potential for overlooking of the nursery playground, compromising child safety. She requests whether consideration could be given to reducing its height or restricting access to the viewing platform to obscure this view.

South Willesborough and Newtown Community Group: has commented that with the passage of time the 68 parking spaces proposed would be totally inadequate to cater for projected visitor numbers resulting in attempted parking in neighbouring residential streets where a controlled parking zone is proposed. It has worked out that the number of visitors per annum over 300 days would be 50,000, 100,000 and 500,000 respectively and that the number of parking spaces required could be as high as 209 spaces towards the end of this period (assuming 50% of visitors travel by car) or 417 spaces towards the end of this period (assuming 100% visitors travel by car).

It also comments that parking will be required for upwards of 60 staff, which will not park on the Klondyke site, as well as parking for sundry contractors. **(SSDM comment:** There would be 10 members of staff and the Transport Assessment assumes 80% will travel to work by car).

It points out that there has been a suggestion of overflow parking into the Designer Outlet Centre but they have stated that their spaces are for shoppers only. **(SSDM comment:** There are likely to be linked trips to both AIMREC and the Designer Outlet Centre.)

A **petition** has been received with 93 signatories raising concerns about the following:

- Extra traffic movements on local roads;
- Congestion already caused by the Designer Outlet;
- Inadequate off-road parking for the proposed 100,000 visitors per annum to the museum;
- The proposed intrusive viewing gallery overlooking of rear of properties in Wainwright Place;
- Noise and disruption from the light steam passenger carrying railway;
- Traffic Lights Control entry and exit to and from the site.

Planning Policy

17. The Development Plan comprises the saved policies in the adopted Ashford Borough Local Plan 2000, the adopted LDF Core Strategy 2008, the adopted Ashford Town Centre Action Area Plan 2010, the Tenterden & Rural Sites DPD 2010, the Urban Sites and Infrastructure DPD 2012 and the Chilmington Green AAP 2013.

18. The relevant policies from the Development Plan relating to this application are as follows:-

Ashford Borough Local Plan 2000

S36 – Klondyke Works

TP6 – Cycle Parking

Local Development Framework Core Strategy 2008

CS1 – Guiding Principles

CS4 – Ashford Urban Area

CS7 – The Economy and Employment Development

CS9 – Design Quality

CS10 – Sustainable Design and Construction

CS11 – Biodiversity and Geological Conservation

CS15 – Transport

CS17 - Tourism

CS20 – Sustainable Drainage

Urban Sites and Infrastructure DPD 2012

U0 - Presumption in Favour of Sustainable Development

19. The following are also material to the determination of this application:-

Supplementary Planning Guidance/Documents

Sustainable Drainage SPD 2010

Sustainable Design and Construction SPD April 2012

Dark Skies SPD 2014

Government Advice

National Planning Policy Framework 2012

20. Members should note that the determination must be made in accordance with the Development Plan unless material considerations indicate otherwise. A significant material consideration is the National Planning Policy Framework (NPPF). The NPPF says that less weight should be given to the policies above if they are in conflict with the NPPF.

Assessment

21. The main issues for consideration are:
- (a) Principle of development
 - (b) Impact on listed buildings
 - (c) Visual Amenity
 - (d) Residential Amenity
 - (e) Interim Junction
 - (f) Traffic and Parking
 - (g) Miniature Railway
 - (h) Biodiversity
 - (i) Trees and landscaping
 - (j) Drainage
 - (k) Carbon Off-setting
 - (l) Whether planning obligations are necessary

Principle of development

22. Saved policy S36 of the Ashford Borough Local Plan 2000 supports the conversion of the railway buildings at Klondyke Works for tourist, leisure or light industrial purposes. It states, in the pre-ambule to the policy, that a tourism use of the site would provide an excellent opportunity to promote the area's heritage and could work well in association with the designer outlet.

23. The buildings have been demolished but the redevelopment of the site for these uses would be acceptable in policy terms. The proposed development for a model railway education centre would therefore be acceptable in terms of its use. It is considered that it would work alongside the designer outlet, providing opportunities for linked trips.
24. The sustainable growth of tourism through the provision of a wide variety of new facilities is also supported by Policy CS17 of the Core Strategy. It is therefore considered that this proposal represents an ideal use for the site in principle.

Impact on listed buildings

25. Saved policy S36 requires that care should be taken in the design and layout of the development to protect the setting of the nearby listed buildings. Furthermore the NPPF states that in determining applications, local authorities should consider the impact on designated heritage assets.
26. The closest listed building is the Grade II listed former school building, now in use as apartments, which abuts the site to the south. It is situated on higher land to the application site; a 3m high embankment with tree screen extending along this boundary. This natural separation and also the fact that the buildings are orientated in different directions onto different streets means that the buildings would not be 'read' as a set piece.
27. A cross section is provided in figure 8 which shows the relationship between this listed former school building and the AIMREC building. The section shows that due to changes in land level, the AIMREC building (excluding the viewing platform) would have a lower ridge height than that of the listed building. The viewing platform itself would be situated at the end of the building furthest from the southern boundary and as such its impact upon the listed building would not be significant. The AIMREC building would be positioned at an angle to the listed building (just under 12m at its closest point) thereby reducing its impact still further. (It is worth noting also that the AIMREC building would be significantly smaller in scale than the Klondyke workshop that formerly occupied the site and it would also be set back further from the southern site boundary.)
28. It is not considered that the proposed development would harm the setting of the listed former school building by virtue of its height, scale or design.
29. The Grade II listed gatehouse is situated on the other side of Newtown Road to the application site on slightly higher land such that its Italianate clock tower would rise above the main part of the AIMREC building. The AIMREC viewing platform would be of a similar height to the clock tower but as it is situated on the rear elevation of the AIMREC building its impact on it would be reduced.

Also, its contemporary form and design are quite different such that it does not 'compete' architecturally with the listed building. Due to the separation of the buildings; the relative heights and different architectural language, it is not considered that the proposed building would harm the setting of the listed gatehouse building.

30. The listed railway sheds on the Newtown Works site are situated further from the proposed AIMREC building and on the other side of Newtown Road. It is not considered that the proposed development would affect or harm the setting of these buildings.
31. In conclusion, it is not considered that the proposed development would be harmful to designated heritage assets.



Figure 8: Relationship to listed former school building

Visual Amenity

32. The use requires a large warehouse type space where models can be displayed. It should be as adaptable as possible with controlled lighting to protect displays. The proposed development provides such a space although its bulk and scale have been reduced by the articulation of the building into three distinct elements and the application of different materials to the different parts of the building. Although, there is limited fenestration to those parts of the building used for display purposes, fenestration in other parts of the building (the café and shop) is maximised to help add interest to elevations.
33. The original schemes, outlined in the Design and Access Statement, showed a more traditional building using reclaimed bricks from the former railway. In response to comments made by Ashford's Regional Design Panel at design review in December 2014, the scheme has evolved. The proposals show a building with a more interesting and contemporary architectural form using a variety of high quality, natural and contemporary materials (the reclaimed bricks are no longer available).
34. The building includes a number of potentially interesting features which would add richness to the design. References to the railways are frequent and found

in some of the architectural detailing: the entrance tunnel is described as being reminiscent of a railway tunnel parapet; cycle storage would use half round train wheels as the locking hoop support for bikes. The brick entrance tunnel with pedestrian bridge over provides a potentially exciting entrance to the building. The applicant has provided some images of what it would look like and details of materials. The detailed design of the entrance tunnel and other features would need to be conditioned as part of any planning permission to ensure that this level of richness is provided.

35. The scheme includes some ambitious design elements, some essential to the acceptability of the scheme in planning terms and others desirable, adding to the richness of the project but arguably not so essential. Officers have sought reassurance from the applicants that these components can be delivered.
36. The pedestrian route through the embankment is considered essential in terms of providing a legible development for pedestrians. The applicants have indicated that some preliminary work has been carried out on this and it is achievable. (NB. Due to the change in land levels and the need to provide steps, level access would be achieved adjacent to the vehicular access.)
37. Such a building would require an interesting entrance and various design approaches could achieve this outcome. The proposed tunnel entrance with bridge over would add to the richness of the building if it is built to a high standard using high quality materials. It is intended to condition this accordingly. The tower adds to the experience of the building and helps create a more interesting form but is arguably less important.
38. Some preliminary work has been carried out on the miniature railway which suggests the level changes are within workable limits and the land-take is realistic. The tunnelling beneath the vehicular access would be achieved using a large standard concrete drainage pipe. The train would be secured within the tunnel at night with gates to either end.
39. It is considered that the building would provide a high quality of design and detailing in accordance with Policy CS9 of the Core Strategy.



Figure 9: Perspective drawing of building entrance

Residential Amenity

40. At its closest point, the AIMREC building would be almost 12m from the closest building in Wainwright Place, the listed former school building now in use as flats, but the impact would be reduced by the angle between the buildings and the articulation of the building. Furthermore, the AIMREC site is at a lower level (by some 3m) to buildings in Wainwright Place and separated by an embankment and tree screen. It is not considered that the proposed development would impact adversely on existing properties in terms of bulk and scale. As it is situated to the north of existing properties there would be no overshadowing of these properties.
41. The potential for overlooking from the AIMREC viewing platform of properties in Wainwright Place including the nursery playground is an issue that has been raised during the consultation by local people. The viewing platform is situated towards the northern end of the building some 60m from the nearest private garden boundary and some 145m from the nursery playground.
42. In response to the concern about overlooking of the nursery playground, the applicant has provided a section drawing which shows that the nursery playground is some 145m away from the viewing platform and beyond the parking areas to both the listed former school building and nursery building. A photo has also been provided which shows a view from a cherry picker - positioned at the same height as the viewing platform - in the direction of the nursery. Given the significant separation distance, the relatively low angle of the line of sight, partial tree screen and intervening parking areas, I do not

consider that the viewing platform would cause harm to residential amenity or the amenity of the nursery playground by virtue of overlooking.

43. It is not considered that the miniature railway would impact upon the amenity of existing residential occupiers in terms of noise, dust or vibration.
44. It is not considered that the proposed development would impact adversely on the amenity of existing residents.

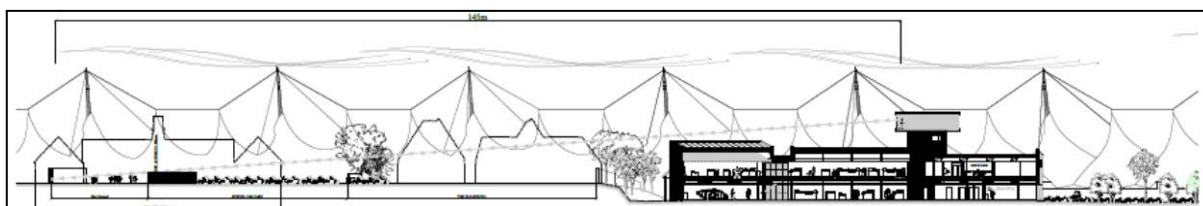


Figure 10: Section showing sight line from viewing platform to nursery

Interim junction design

45. The proposal requires the provision of an interim signalised junction at the entrance into the site with the final junction design delivered once the housing development to the north comes on stream. The applicants' safety audit identified an issue in relation to this junction as regards traffic leaving the Newtown works site and the highway authority initially raised objections on safety grounds. This has since been amended with the Newtown works access signalised as part of the signal design. The highway authority is satisfied that the amended design raises no highway safety concerns.

Traffic and Parking

46. The traffic assessment concludes that the proposed development would generate traffic largely outside of the commuter peak period and hence not cause any adverse impact on the local road network especially at commuter peak periods.
47. The traffic assessment is based upon the assumption that 50% of the visitors would arrive by car, 30% by rail and the remaining 20% by coach. Based upon this assumption, the assessment shows that the evening commuter peak period (17:00 – 18:00), the proposal is expected to generate 21 vehicles (two-way) in the year of opening and 52 in a future year. The development peak will be on Saturday between 15:00 – 16:00 hours with 56 vehicles (two way) in the year of opening and up to 144 in 10 years. As a tourist destination, the vehicle trips to/from the site are expected to arrive primarily from the M20.

48. Following consultation with KCC Highways and Transportation, a sensitivity test has been undertaken based on an assumption that 60% of the visitors would arrive by car, 25% by rail and the remaining 15% by coach. Based upon this assumption, the assessment shows that the evening commuter peak period (17:00 – 18:00), the proposal is expected to generate 23 vehicles (two- way) in a year of opening and 62 in a future year. The development peak hour will be Saturday between 15:00 – 16:00 hours with 65 vehicles (two- way) in the year of opening and up to 172 in 10 years.
49. The traffic impact assessment considers all the committed development traffic for all known major committed development in 2015 ie. Newtown Works (Phase 1), Waterbrook, Park Farm and Cheeseman's Green.
50. It demonstrates that the maximum increase in two-way traffic of 4.1% could occur on Newtown Road (in the vicinity of the site) during Saturday peak hour. Based upon the sensitivity test, this increase in traffic is 4.7%.
51. The proposed traffic signalled access junction has been analysed using LINSIG and the analyses (both original traffic assumption and sensitivity test) shows that the proposed signal junction would operate satisfactorily during 2016 and 2026 scenarios with the development in place.
52. The assessments of Romney Marsh Road/Newtown Road junction show that the increase in queues as a result of the proposed development on the junction would not be significant when compared to the base plus committed development.
53. The South Willesborough and Newtown Community Group has raised concerns about the inadequacy of parking both now and in future years as visitor numbers increase and people attempting to park in neighbouring streets where a Controlled Parking Zone (CPZ) is proposed by Ashford Borough Council.
54. KCC Highways and Transportation has confirmed that the proposed parking is in line with their standards set out in Supplementary Planning Guidance Note 4 (SPG4). The standards state that a use class D1 which is used as a place of entertainment as well as education facility would require 1 car parking space per 60m². Therefore, the proposal for the model railway education centre with a total gross floor area of 3,000m² would require 50 parking spaces. The provision of 68 spaces is therefore in line with the parking standards, and exceeds them by some considerable margin.
55. The Transport Assessment (sensitivity test) shows that the maximum accumulation will be 65 spaces, between 2pm and 3pm on a weekday by the 10th year of operation which is when the estimated projection of 400,000 visitors a year will be reached (2026).

56. The highway authority has advised that the proposed framework travel plan should be subject to a Section 106 Agreement so that it can effectively monitor the travel plan. There will need to be annual surveys over a 5 year period so that the travel plan can be monitored effectively. The highway authority has also advised that the proposed car park should be subject to a car parking management condition on any planning permission granted. It is likely that due to overspill parking issues associated with the Designer Outlet the car park will need to be subject to a pay and display scheme in which visitors can obtain a refund if they purchase a ticket into the centre. An appropriate condition is proposed to cover this issue.
57. The proposed CPZ for Newtown will help address the existing parking situation in Newtown where there are currently no controls and people working in the outlet/using the railway can park there all day. A contribution towards the proposed CPZ is sought as part of this application.
58. Buses - Negotiations are currently underway on improvements to the bus service in Newtown Road together with the provision of replacement bus stops in connection with the McArthur Glen designer outlet centre planning application which was considered by members earlier this year. The new bus stops will be provided to either side of Newtown Road immediately to the west of the Newtown Road railway bridge and will be convenient for visitors to the AIMREC site also. The enhanced bus service would improve public transport links across the area to both the Designer outlet and AIMREC buildings.

Miniature Railway

59. The miniature railway would circumnavigate the site passing beneath the vehicular access by way of a 50m tunnel with a diameter of 2m. It would have a gauge width of 184mm and track width of 529mm. The train would move clockwise around the site passing along the bottom of the embankment at the southern site boundary and crossing the replica iron bridge.
60. KCC Highways and Transportation has indicated that the railway along the boundary with Newtown Road would require structural approval as it is within 3.65 metres of the public highway. The applicants have been advised to contact the Structures Team at KCC Highways and Transportation to discuss the proposals in more detail.

Biodiversity

61. The Ecological Appraisal identified habitats currently within the development site to be of low value for reptiles. However, it states that reptiles may be able to migrate between the development site and adjacent habitats via railway bankside vegetation and identified the need for a Reptile Survey. The

applicants have instructed this survey to be carried out (to commence in April of this year).

62. It is not usual to condition ecological surveys as the mitigation will depend upon the outcome of the survey work and KCC Ecology do not advise this approach. It is therefore requested of members of the Planning Committee that this matter is deferred to officers to resolve under the delegated agreement prior to a decision being issued on the planning application.

Trees and Landscaping

63. The Ashford Borough Plan requires that the existing tree screens on the southern and eastern boundaries of the site should be retained (pre-amble to policy S36). The applicant has confirmed that these trees would be retained and the bank unaffected by the proposals with the route of the miniature train extending along the level part of the site beneath the bank.
64. The landscaping proposals for the site would provide additional green areas to the boundaries of the site as well as provide a picnic area to the front of the building and help break up the expanse of parking with some tree planting.

Drainage

65. Attenuation is to be provided via a combined SUDS approach incorporating tanks (less favourable) and permeable paving (more favourable) and introducing green areas. Due to this being a redevelopment of an existing developed area these methodologies are considered acceptable in a restrictive urban setting.
66. The applicant is seeking significant betterment on the site by seeking to substantially reduce the runoff from the site in line with the Ashford SUD SPD. The Project Office has commented that whilst this is above the 'best endeavours' rate of 4l/s/ha, the 5l/s proposed discharge rate is considered a substantial betterment in comparison to the current drainage regime on the site which is considered to discharge at an uncontrolled rate.

Carbon Off-Setting

67. As the development has a gross external floor area of over 1000sq.m it is required to be carbon neutral in line with Policy CS20 of the Core Strategy with any shortfall met by financial contributions to enable residual carbon emissions to be offset through a local project.

Planning Obligations

68. Regulation 122 of the Community Infrastructure Regulations 2010 says that a planning obligation may only constitute a reason for granting planning permission for a development if the obligation is:
- (a) necessary to make the development acceptable in planning terms,
 - (b) directly related to the development; and
 - (c) fairly and reasonably related in scale and kind to the development
69. Policies CS1, CS2, CS15 and CS18, Urban Sites and Infrastructure DPD Policy U24, Kent Local Transport Plan and guidance in the NPPF provide clear policy support for seeking financial contributions towards bus service contributions. Members will be aware however of negotiations well underway with McArthur Glen on the S106 Agreement for the Designer Outlet Centre in respect of contributions towards an enhanced bus service on Newtown Road and the provision of new bus stops adjacent to Newtown Railway bridge. As this service/stops would also serve the AIMREC building, no additional contributions are sought in this instance.
70. I recommend the planning obligations in Table 1 be required should the Committee resolve to grant permission. I have assessed them against Regulation 122 and for the reasons given consider they are all necessary to make the development acceptable in planning terms, are directly related to the development and are fairly and reasonably related in scale and kind to the development. Accordingly, they may be a reason to grant planning permission in this case.

Table 1

	Planning Obligation			Regulation 122 Assessment
	Detail	Amount(s)	Trigger Point(s)	
1.	<p><u>Carbon Off-Setting Contribution</u></p> <p>Contribution for funding carbon savings based on the residual carbon emissions of the building set out in the approved energy performance certificate and quantified over 10 years.</p> <p>Project: Retrofitting of International House with Low and Zero Carbon Technologies</p>	<p>To be calculated using the shadow price of carbon set out in the Sustainable Design and Construction SPD</p>	<p>Payable on the occupation of the building</p>	<p>Necessary in order to ensure the development is carbon neutral pursuant to Core Strategy policies CS1, and CS10 (C), the Sustainable Design and Construction SPD and guidance in the NPPF.</p> <p>Directly related as only carbon emissions from this development would have to be off-set.</p> <p>Fairly and reasonably related in scale and kind as off-setting would not be required in the absence of carbon emissions from this development and any payment is based on the amount of carbon dioxide to be offset.</p>

1.22

	Planning Obligation			Regulation 122 Assessment
	Detail	Amount(s)	Trigger Point(s)	
2.	<p><u>Controlled Parking Zone</u></p> <p>Contribution towards the making and implementation of a Traffic Regulation Order in Newtown and South Ashford area Zone 11 (north of Norman Road and east of Beaver Road)</p>	<p>To be agreed in consultation with KCC</p>	<p>On first occupation of the building</p>	<p>Without a scheme, visitors to (and staff employed at AIMREC may otherwise seek to park on the roads in the nearby Newtown and South Ashford area Zone 11 (north of Norman Road and east of Beaver Road) to avoid parking charges. The contribution is necessary pursuant to policy CS1 of the Core Strategy in order to protect residential amenity, maintain highway safety through preventing inappropriate parking and to support measures in the required Travel Plan to achieve a shift in travel behaviour.</p> <p>Directly related as Newtown is only a short distance from the AIMREC building.</p> <p>Fairly and reasonably related in scale and kind considering the scale of the development as proposed and the related number of vehicle trips</p>

	Planning Obligation			Regulation 122 Assessment
	Detail	Amount(s)	Trigger Point(s)	
3.	<p><u>Monitoring Fee</u></p> <p>Contribution towards the Council's costs of monitoring compliance with the agreement or undertaking</p>	£1000 one –off payment	Payment upon commencement of development	<p>Necessary in order to ensure the planning obligations are complied with.</p> <p>Directly related as only costs arising in connection with the monitoring of the development and these planning obligations are covered.</p> <p>Fairly and reasonably related in scale and kind considering the extent of the development and the obligations to be monitored.</p>
4.	<p><u>Off-site Highway Works</u></p> <p>Provision of interim junction</p>	To be agreed in consultation with KCC		<p>Necessary in order to meet the demand generated by the development and in the interests of highway safety pursuant to Core Strategy policies CS1, CS2, CS15 and CS18, Urban Sites and Infrastructure DPD policy U24 (if applicable), Kent Local Transport Plan and guidance in the NPPF.</p>

	Planning Obligation			Regulation 122 Assessment
	Detail	Amount(s)	Trigger Point(s)	
				<p>Directly related as occupiers will travel and the facilities to be funded will be available to them.</p> <p>Fairly and reasonably related in scale and kind considering the extent of the development and because the amount has been calculated based on the scale of the development and the estimated number of relevant trips.</p>
5.	<p><u>Travel Plan</u></p> <p>Submit a travel plan to be approved by the Council which aims to reduce car use by occupiers and encourage alternative modes of transport. Implementation of the approved travel plan.</p>	Not applicable	Travel plan to be approved prior to first occupation	<p>Necessary in order to promote public transport and encourage a modal shift away from private car use pursuant to Core Strategy policies CS1, CS2, CS15 and CS18, Kent Local Transport Plan and guidance in the NPPF.</p> <p>Directly related as occupiers will travel and the measures to be implemented through the travel plan will be available to them.</p>

Planning Obligation			Regulation 122 Assessment
Detail	Amount(s)	Trigger Point(s)	
			Fairly and reasonably related in scale and kind considering the extent of the development and because the amount has been calculated based on the scale of the development.
6.	<u>Travel Plan Monitoring Fee</u> Contribution towards the cost of monitoring compliance with the travel plan and helping to ensure its success	£5,000	<p>£1,000 upon first occupation and £1,000 on the anniversary thereof for 4 years</p> <p>Necessary in order to ensure public transport is promoted and the modal shift envisaged by the travel plan takes place pursuant to Core Strategy policies CS1, CS2, CS15 and CS18, Kent Local Transport Plan and guidance in the NPPF.</p> <p>Directly related as the travel plan needs to be implemented in order to ensure the development is sustainable.</p> <p>Fairly and reasonably related in scale and kind considering the extent of the development and the monitoring to be carried out.</p>

Planning Obligation			Regulation 122 Assessment
Detail	Amount(s)	Trigger Point(s)	
<p> Notices will have to be served on the Council at the time of the various trigger points in order to aid monitoring. All contributions to be index linked as set out on the council web site in order to ensure the value is not reduced over time. The costs and disbursements of the Council's Legal Department incurred in connection with the negotiation, preparation and completion of the deed are payable. The Kent County Council may also require payment of their legal costs. </p> <p> If an acceptable agreement/undertaking is not completed within 3 months of the committee's resolution to grant, the application may be refused. </p>			

Human Rights Issues

71. I have also taken into account the human rights issues relevant to this application. In my view the “Assessment” section above and the Recommendations below represent an appropriate balance between the interests and rights of the applicant (to enjoy his land subject only to reasonable and proportionate controls by a public authority) and the interests and rights of those potentially affected by the proposal (to respect for private life and the home and peaceful enjoyment of their properties).

Working with the applicant

72. In accordance with paragraphs 186 and 187 of the NPPF Ashford Borough Council (ABC) takes a positive and proactive approach to development proposals focused on solutions. ABC works with applicants/agents in a positive and proactive manner as explained in the note to the applicant included in the recommendation below.

Conclusion

73. Development Plan Policy supports the use of the former Klondyke Works site for tourist, leisure or light industrial purposes, stating in the pre-ambule to the policy that a tourism use of the site would be an excellent opportunity to promote the area’s heritage and could work well in association with the designer outlet. The proposed development of a model railway museum on this former railway site and adjacent to existing railway land represents an ideal use of this derelict site.
74. The applicants have demonstrated that the scheme would not be harmful to designated heritage assets including the Grade II listed former school building and gatehouse building which are separated from the AIMREC site by a significant level change and main road respectively. The viewing platform to the AIMREC building is situated on the far side of the building to these listed buildings such that its impact is reduced. The different form and architectural language of the AIMREC building help ensure that it does not ‘compete’ with the clock-tower to the gate house.
75. The proposed scheme would be of an interesting design and layout designed to provide a visitor attraction for the town and engage visitors in the history of the railways. It includes some architecturally ambitious elements and richness of detail (which will need to be controlled by condition). It would not be overbearing or result in unacceptable levels of overlooking of neighbours either from the building itself or from the viewing tower.
76. The proposed development makes provision for an interim junction to provide safe access into the site. Traffic impacts on the road network are within acceptable limits and the parking is in line with KCC’s Parking Standards.

Although the ecological appraisal identified habitat within the site as being of low value for reptiles, it does nevertheless recommend that a reptile survey is carried out. This is currently underway and the outcome of this survey will be considered prior to the decision being issued so that appropriate mitigation can be identified in line with KCC advice. In terms of site drainage, the proposal would result in significant betterment on the current situation, through a combined SUDS approach which includes permeable paving and green areas.

77. Given the above, the planning application is considered acceptable and as such it is recommended that the planning application is approved.

Recommendation

- (A) The prior receipt of a Reptile Survey and appropriate mitigation**
- (B) Subject to the applicant first entering into a section 106 agreement in respect of planning obligations related to**
- a. Carbon off-setting; controlled parking zone; off-site highway works, travel plan and travel plan monitoring fee.**
 - b. Monitoring fee**
as detailed in table 1, in terms agreeable to the Strategic Sites and Design Manager or the Development Control Manager in consultation with the Head of Legal and Democratic Services, with delegated authority to either the Strategic Sites and Design Manager or the Development Control Manager to make or approve minor changes to the planning obligations and planning conditions, as they see fit.
- (C) Permit**

Subject to the following conditions and notes:

1. The development hereby permitted shall be begun before the expiration of 3 years from the date of this decision.

Reason: To comply with the requirements of Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Written details including source/ manufacturer, and samples of bricks, tiles and cladding materials to be used externally shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced and the development shall be carried out using the approved external materials.

Reason: In the interests of visual amenity.

3. Prior to the commencement of the development hereby permitted the following details shall be submitted to and approved by the Local Planning Authority in writing:
 - (a) 1:20 sectional details and vertical and horizontal sections through windows and entrances;
 - (b) 1:50 scale drawings of architectural features to include main building entrance feature and viewing platform;
 - (c) 1:20 sectional and elevational details of facade/metal screen to middle block of building (front and rear elevations);
 - (d) Details of Iron Bridge replica entrance feature into site.

Reason: So the Local Planning Authority can be satisfied as to the details of the proposal.

4. No construction activities shall take place, other than between 0730 to 1800 hours (Monday to Friday) and 0730 to 1300 hours (Saturday) with no working activities on Sunday or Bank Holiday.

Reason: To protect the amenity of local residents in accordance with Policy CS1 of the Local Development Framework Core Strategy.

5. No development shall take place until a Construction Management Plan has been submitted to and approved by the Local Planning Authority in writing. This plan should include:
 - (a) Routing of construction and delivery vehicles to / from the site
 - (b) Parking and turning areas for construction and delivery vehicles and site personnel, with the replacement of staff parking for the duration of construction;
 - (c) Timing of deliveries;
 - (d) Provision of wheel washing facilities;
 - (e) Details of site access point(s) for construction;
 - (f) Temporary traffic management/signage.

The development should be carried out in accordance with the Construction Management Plan.

Reason: In the interests of highway safety and the amenities of neighbouring residents.

6. Prior to the use commencing, the vehicle parking spaces shown on drawing number 13.115.51 Rev A shall be provided and permanently retained for the duration of the development.

Reason: To ensure the provision and retention of adequate off-street parking facilities for vehicles in the interest of highway safety.

7. Prior to the use commencing, the vehicle loading/unloading and turning facilities shown on drawing number 13.115.52.Rev A shall be provided and permanently retained for the duration of the development.

Reason: In the interests of highway safety.

8. Prior to the use commencing, the cycle parking facilities shown on drawing number 13.115.52.Rev A shall be provided and permanently retained for the duration of the development.

Reason: To ensure the provision and retention of adequate off-site parking facilities for bicycles in the interest of highway safety.

9. Prior to the use commencing, the access details shown on drawing number 13.115.51 Rev A and drawing number 2905-01 Rev D shall be completed and maintained for the duration of the development.

Reason: In the interests of highway safety.

10. Prior to the use commencing, the associated highway works shown on drawing number 13.115.51 Rev A and drawing number 2905-01 Rev D shall be completed and maintained for the duration of the development.

Reason: In the interests of highway safety.

11. No development shall take place until details of measures to prevent the discharge of surface water onto the highway have been submitted to and approved in writing by the Local Planning Authority.

Reason: So the Local Planning Authority can be satisfied as to the details of the proposal.

12. Prior to the use commencing, details of new pedestrian wayfinding signs signing the proposed Model Railway Education Centre from the International Station shall be submitted to and approved in writing by the Local Planning Authority. The signs shall be provided in situ before the use commences.

Reason: To ensure pedestrian legibility across the area in the interests of good planning.

13. No development shall take place until details of a parking management scheme have been submitted to and approved by the Local Planning Authority in writing. These details should include the provision of a Pay and Display Scheme in which visitors can obtain a refund if they purchase a ticket into the centre. The Pay and Display Scheme shall be implemented prior to the opening of the AIMREC building.

Reason: To prevent overspill parking from the Designer Outlet in the interest of the residential amenity of the area.

14. Prior to the commencement of development, a detailed remediation scheme to ensure that the site is suitable for the intended use (by removing unacceptable risks to human health, buildings and other property and the natural and historical environment) must be submitted to and approved in writing by the Local Planning Authority. The scheme must describe all the relevant works to be undertaken including, the proposed remediation objectives and performance criteria, a schedule of works and site management protocols.

The scheme must deliver a site that will not qualify as 'contaminated land' under Part 2A of the Environmental Protection Act 1990, having regard to the intended use of the land after remediation.

The approved scheme shall thereafter be carried out in accordance with its approved terms, unless otherwise agreed in writing by the Local Planning Authority. The Local Planning Authority must be notified at least two weeks prior to commencement of the remediation scheme works.

Following completion of the remediation scheme, a verification report that demonstrates the effectiveness of the remediation carried out must be prepared and submitted for approval in writing by the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

15. If unexpected contamination is found at any time when carrying out the approved development it must be reported in writing to the Local Planning Authority. An investigation and risk assessment must be undertaken in accordance with the requirements of condition 1, and where remediation is necessary a remediation scheme must be prepared in accordance with the requirements of condition 2.

Following completion of the remediation scheme a verification report that demonstrates the effectiveness of the remediation carried out must be prepared and submitted for approval in writing by the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors. (LDF Core Strategy Policy CS1 and CS4)

16. No external lighting shall be installed until details have been submitted to, and approved in writing by the Local Planning Authority prior to the commencement of the development. This submission shall include a layout plan with beam orientation and a schedule of light equipment proposed (luminaire type; mounting height; aiming angles and luminaire profiles). The approved scheme shall be installed, maintained and operated in accordance with the approved details unless the Local Planning Authority gives its written consent to the variation.

Reason: To protect the appearance of the area and local residents from light pollution.

17. No development shall take place until full details of both hard and soft landscape works have been submitted to and approved in writing by the Local Planning Authority and these works shall be carried out as approved. These details shall be in accordance with the approved landscape masterplan (drawing no 3130_DR_001). They should include proposed finished levels or contours; means of enclosure; car parking layouts; other vehicle and pedestrian access and circulation areas; hard surfacing materials; minor artefacts and structures (e.g. furniture, picnic area, refuse or other storage units, signs, lighting etc); proposed and existing functional services above and below ground (e.g. drainage, power communications cables, pipelines etc indicating lines, manholes, supports etc).

Reason: In order to protect and enhance the amenity of the area.

18. The details of soft landscape works required in condition 17 above shall include planting plans; written specifications (including cultivation and other

operations associated with plant and grass establishment); schedules of plants, noting species, plant sizes and proposed numbers/densities where appropriate; and an implementation programme.

Reason: To ensure that adequate details of the proposals are submitted in the interests of the protection and enhancement of the area.

19. All hard and soft landscape works shall be carried out in accordance with the approved details. The works shall be carried out prior to the occupation of any part of the development or in accordance with the programme agreed in writing with the Local Planning Authority; and any trees or plants whether new or retained which within a period of 5 years from the completion of the development die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species, unless the Local Planning Authority gives written consent to any variation.

Reason: In the interests of the amenity of the area.

20. The approved development shall be carried out in such a manner as to avoid damage to the existing trees along site boundaries, including their root systems, and other planting to be retained by observing the following:

(a) All trees to be preserved shall be marked on site and protected during any operation on site by temporary fencing in accordance with BS 5837:2012, (Trees in relation to design, demolition and construction - recommendations). Such tree protection measures shall remain throughout the period of construction;

(b) No fires shall be lit within the spread of branches or downwind of the trees and other vegetation;

(c) No materials or equipment shall be stored within the spread of the branches or Root Protection Area of the trees and other vegetation;

(d) No roots over 50mm diameter shall be cut, and no buildings, roads or other engineering operations shall be constructed or carried out within the spread of the branches or Root Protection Areas of the trees and other vegetation;

(e) Ground levels within the spread of the branches or Root Protection Areas (whichever the greater) of the trees and other vegetation shall not be raised or lowered in relation to the existing ground level, except as may be otherwise agreed in writing by the Local Planning Authority.

(f) No trenches for underground services shall be commenced within the Root Protection Areas of trees which are identified as being retained in the approved plans without the prior written consent of the Local Planning Authority. Such trenching as might be approved shall be carried out to National Joint Utilities Group recommendations.

Reason: Pursuant to Section 197 of the Town and Country Planning Act 1990 and to protect and enhance the appearance and character of the site and locality in accordance with Policy EN1 of the Local Plan.

21. No development shall take place until details of boundary treatment have been submitted to and approved in writing by the Local Planning Authority. This should include a plan indicating the positions, design, materials and type of boundary treatment to be erected. The boundary treatment shall be completed before the building is occupied. Development shall be carried out in accordance with the approved details and shall be permanently maintained.

Reason: In the interest of the visual amenity of the area.

22. Prior to development commencing, a scheme for the enhancement of biodiversity on the site in accordance with the general recommendations set out in the Ecological Appraisal (Lloyd Bore) shall have been submitted to and approved in writing by the Local Planning Authority. The approved scheme shall take account of any protected species that have been identified on the site, and in addition shall have regard to the enhancement of biodiversity generally. It shall be implemented in accordance with the approved proposals within it and shall be carried out in perpetuity unless otherwise agreed in writing by the Local Planning Authority.

Reason: To protect and enhance existing species and habitat on the site in the future

23. The premises shall not be open to the public other than between the hours of 10:00AM – 5:00PM Monday to Sunday with the exception of Christmas Day, Boxing Day and New Year's Day when the premises will be closed.

Reason: To protect the residential amenity of the locality

24. The building hereby approved shall be constructed to achieve a minimum Very Good Building Research Establishment BREEAM (or subsequent equivalent quality assured scheme) overall standard comprising the following minimum elements:

i) 'Excellent' standard in respect of energy credits

- ii) 'Maximum' standard in respect of water credits
- iii) 'Excellent' standard in respect of material credits

Prior to development commencing, the following details shall be submitted to the Local Planning Authority for written approval:

1. Details of a 'Design Stage' assessment and related certification, and,
2. Details of how the development will reduce carbon dioxide emissions to a level 20% below the predicted total energy demand through the use of on-site sustainable energy technologies such as renewables and/or low carbon technologies.

Following completion of the final building, a BREEAM 'Post Construction Stage' assessment and related certification confirming the BREEAM standard that has been achieved, and stating the amount of residual carbon emissions and how they are proposed to be dealt with to ensure that the development is carbon neutral (including details of any necessary mechanisms to be put in place and associated timetables) shall be submitted to and approved in writing by the Local Planning Authority.

Following any approval of a 'Post Construction State' assessment, the approved measures and technologies to achieve the BREEAM (good/very good/excellent) standard and to ensure that development is carbon neutral shall be implemented in accordance with the approval and thereafter shall be retained in working order in perpetuity.

Reason: In order to ensure that the energy efficiency through sustainable design and construction is achieved

25. No development shall commence until plans and particulars of a sustainable drainage system (including the details below) for the disposal of the site's surface water have been submitted to and approved in writing by the Local Planning Authority.

The final drainage plan for the scheme will be approved by Ashford Borough Council to ensure that surface water runoff from the site is being dealt with appropriately and in line with Ashford Borough Council's Sustainable Drainage SPD. This will include a modified surface water drainage strategy which satisfies the requirements of the SPD.

The submitted system shall comprise retention or storage of the surface water on-site or within the immediate area in a way which is appropriate to the site's location, topography, hydrogeology and hydrology.

Surface water runoff should be dealt with within the application boundary via suitable methods approved by Ashford Borough Council where possible. Proposals should identify any overland flow paths, channelling of flows, or piped flows along with the final point of discharge of the water from the site should be identified.

Permission for discharging of surface water into the existing public sewer must be obtained by the applicant via written confirmation from Southern Water of their agreement to the proposals.

The submitted system shall be designed to (i) avoid any increase in flood risk, (ii) avoid any adverse impact on water quality, (iii) achieve a reduction in the run-off rate in accordance with the Ashford Borough Council Sustainable Drainage SPD document, adopted October 2010. (iv) promote biodiversity, (v) enhance the landscape, (vi) improve public amenities, (vii) return the water to the natural drainage system as near to the source as possible and (viii) operate both during construction of the development and post-completion.

The submitted details shall include identification of the proposed discharge points from the system, a timetable for provision of the system and arrangements for future maintenance (in particular the type and frequency of maintenance and responsibility for maintenance).

The approved system shall be provided in accordance with the approved timetable. The approved system shall be maintained in accordance with the approved details and shall be retained in working order until such time as the development ceases to be in use.

A plan indicating the routes flood waters will take should the site experience a rainfall event that exceeds the design capacity of the surface water drainage system or in light of systems failure (Designing for exceedance) including appropriate mitigation measures and emergency response procedures.

Reason: In order to reduce the impact of the development on flooding, manage run-off flow rates, protect water quality and improve biodiversity and the appearance of the development pursuant to Core Strategy Policy CS20 Sustainable Drainage.

26. Prior to the commencement of development the applicant, or their agents or successors in title, has secured the implementation of
- i archaeological field evaluation works in accordance with a specification and written timetable which has been submitted to and approved by the Local Planning Authority; and

- ii following on from the evaluation, any safeguarding measures to ensure preservation in situ of important archaeological remains and/or further archaeological investigation and recording in accordance with a specification and timetable which has been submitted to and approved by the Local Planning Authority

Reason: To ensure that features of archaeological interest are properly examined and recorded.

Reason: Pursuant to Articles 35 (1) and (2) of the Town and Country Planning (Development Management Procedure) (England) Order 2015, the Local Planning Authority is fundamental to the development permitted that such details must be submitted prior to the works, other than demolition works, commencing on site. This is because, at the time of granting permission, full details were not yet available but this information is necessary to ensure appropriate assessment of the archaeological implications of any development proposals and the subsequent mitigation of adverse impacts through preservation in situ or by record.

27. Prior to the commencement of development the applicant, or their agents or successors in title, has secured the implementation of a programme of archaeological interpretation work in accordance with a written specification and timetable which has been submitted to and approved by the Local Planning Authority.

Reason: To ensure that information of railway heritage archaeological interest is properly disseminated in accordance with NPPF.

28. The development approved shall be made available for inspection, at a reasonable time, by the local Planning authority to ascertain whether a breach of planning control may have occurred on the land (as a result of departure from the plans hereby approved and the specific terms of this permission/consent/approval).

Reason: In the interests of ensuring the proper planning of the locality, the protection of amenity and the environment, securing high quality development through adherence to the terms of planning approvals and to ensure community confidence in the operation of the planning system.

29. The development shall be carried out in accordance with the plans listed in the section of this decision notice headed Plans/Documents Approved by this decision, unless otherwise agreed by the Local Planning Authority.

Reason: To ensure the development is carried out in accordance with the approval and to ensure the quality of development indicated on the approved plans is achieved in practice.

Notes to Applicant

1. The proposed railway along the boundary with Newtown Road will require structural approval as it is within 3.65m of the public highway. The applicant should therefore contact the Structures Team at KCC Highways and Transportation to discuss the proposals in more detail.
2. Planning permission does not convey any approval for construction of the required vehicular crossing, or any other works within the highway for which a statutory licence must be obtained. Applicants should contact Kent County Council - Highways and Transportation
web:www.kent.gov.uk/roads_and_transport.aspx or telephone: 03000 418181) in order to obtain the necessary Application Pack.

It is the responsibility of the applicant to ensure, before the development hereby approved is commenced, that all necessary highway approvals and consents where required are obtained and that the limits of highway boundary are clearly established in order to avoid any enforcement action being taken by the Highway Authority. The applicant must also ensure that the details shown on the approved plans agree in every aspect with those approved under such legislation and common law. It is therefore important for the applicant to contact KCC Highways and Transportation to progress this aspect of the works prior to commencement on site.

3. A formal application for connection to the public sewerage system is required in order to service this development, please contact Southern Water, Sparrowgrove House, Sparrowgrove, Ottersbourne, Hampshire SO21 2SW (tel: 0330 303 0119) or www.southernwater.co.uk
4. Working with the applicant

In accordance with paragraphs 186 and 187 of the NPPF Ashford Borough Council (ABC) takes a positive and proactive approach to development proposals focused on solutions. ABC works with applicants/agents in a positive and proactive manner by;

- offering a pre-application advice service,
- as appropriate updating applicants/agents of any issues that may arise in the processing of their application
- where possible suggesting solutions to secure a successful outcome,
- informing applicants/agents of any likely recommendation of refusal prior to a decision and,

- by adhering to the requirements of the Development Management Customer Charter.

In this instance:

- the applicant/agent was updated of any issues after the initial site visit,
- was provided with pre-application advice,
- the applicant/ agent responded by submitting amended plans, which were found to be acceptable and permission was granted
- The applicant was provided the opportunity to submit amendments to the scheme/ address issues.
- The application was dealt with/approved without delay.
- The application was considered by the Planning Committee where the applicant/agent had the opportunity to speak to the committee and promote the application.

Background Papers

All papers referred to in this report are currently published on the Ashford Borough Council web site (www.ashford.gov.uk). Those papers relating specifically to this application may be found on the [View applications on line](#) pages under planning application reference 15/01575/AS.

Contact Officer: Katy Magnall

Telephone: (01233) 330259

Email: kmagnall@ashford.gov.uk